

E Lewelling Blvd Improvement Project

Meekland Ave to Langton Way

Tuesday, February 26, 2019
Presentation at 6 PM

Ashland Community Center
1530 167th Avenue
San Leandro, CA 94578



Aerial view of E. Lewelling Blvd., looking east

Community Meeting #1



Public Works Agency
— Alameda County —

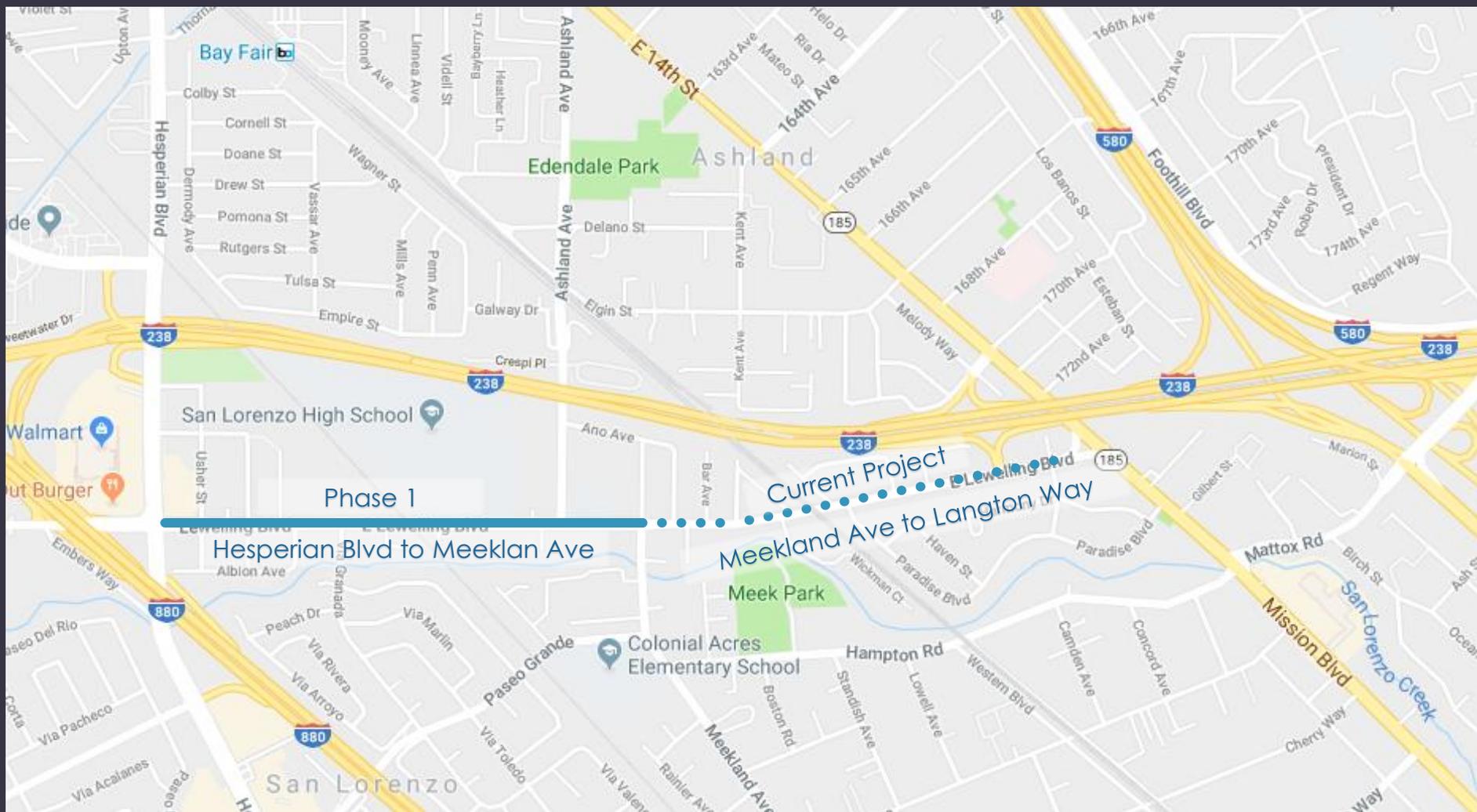
Agenda

- ❖ Introduction
- ❖ Purpose of this Meeting
- ❖ Existing Conditions
- ❖ Improvement Alternatives
- ❖ Community Discussion

Purpose of this Meeting

- ❖ Starting point for project
 - Project information and discussion
- ❖ Public Involvement
 - Community feedback on potential improvement alternatives
 - Share ideas on how East Lewelling Blvd can be improved





E Lewelling Blvd - Meekland Ave to Langton Way

Existing Conditions

- ❖ Sidewalk - none, narrow, or poor condition
- ❖ No bike facility (e.g. bike lanes)
- ❖ Ponding water - needs curb & gutter, drainage improvements
- ❖ Poor pavement condition
- ❖ Lack of standard design features (e.g. parking, driveways, etc.)
- ❖ No walkway across the railroad



This project is intended to:

- ❖ Construct concrete sidewalk, curb & gutter
- ❖ Install bicycle improvements
- ❖ Improve walkability
 - Eliminate parking in pedestrian paths
 - Standardize driveway access
 - Install walkway across the railroad tracks
- ❖ Improve drainage
- ❖ Repair pavement

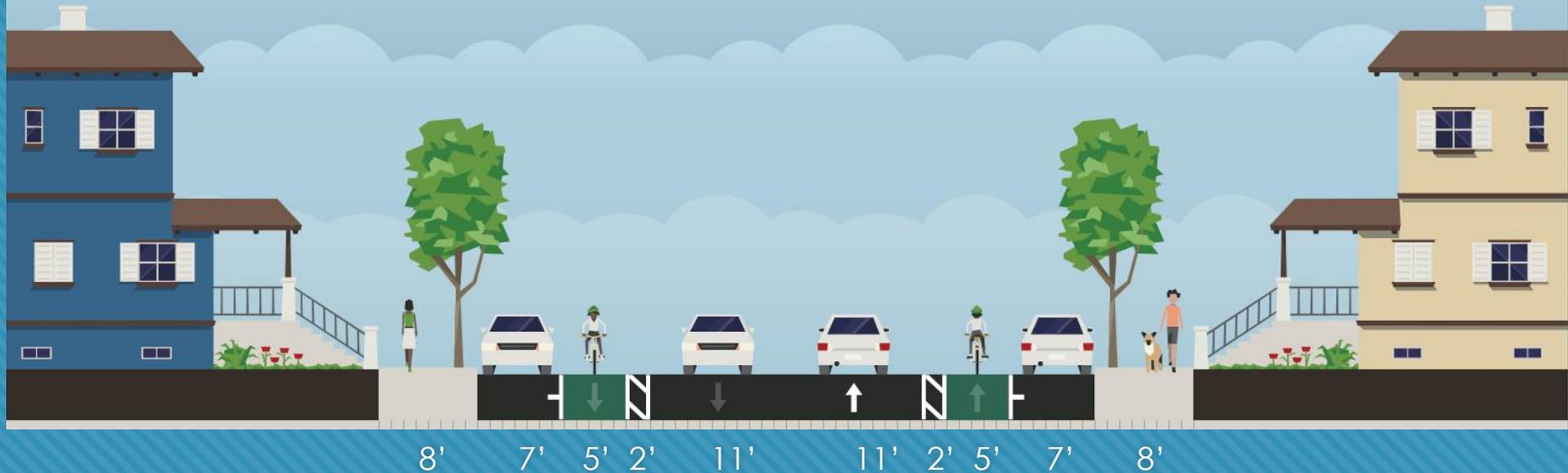


Potential Project Elements

- ❖ Sidewalk (alternatives vary in width)
- ❖ Class II bike lanes with or without buffer
- ❖ On-street parking
- ❖ Center two-way left turn lane
- ❖ Landscape & Hardscape
 - Raised median
 - Similar color scheme as Phase 1 (Hesperian Blvd. to Meekland Ave.)



Right-of-way width: 66 ft.



- Bike lane
- Bike lane buffer
- Sidewalk (8 ft. wide)
- On-street parking
- Center Two-Way Left Turn Lane
- Raised Median

This typical section shows the project features for Alternative #1. The dimensions shown are preliminary and may change to accommodate local site conditions.

Alternative #1 bike lane buffer



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Right-of-way width: 66 ft.



- Bike lane
- Bike lane buffer
- Sidewalk (9 ft. wide)
- On-street parking
- Center Two-Way Left Turn Lane
- Raised median

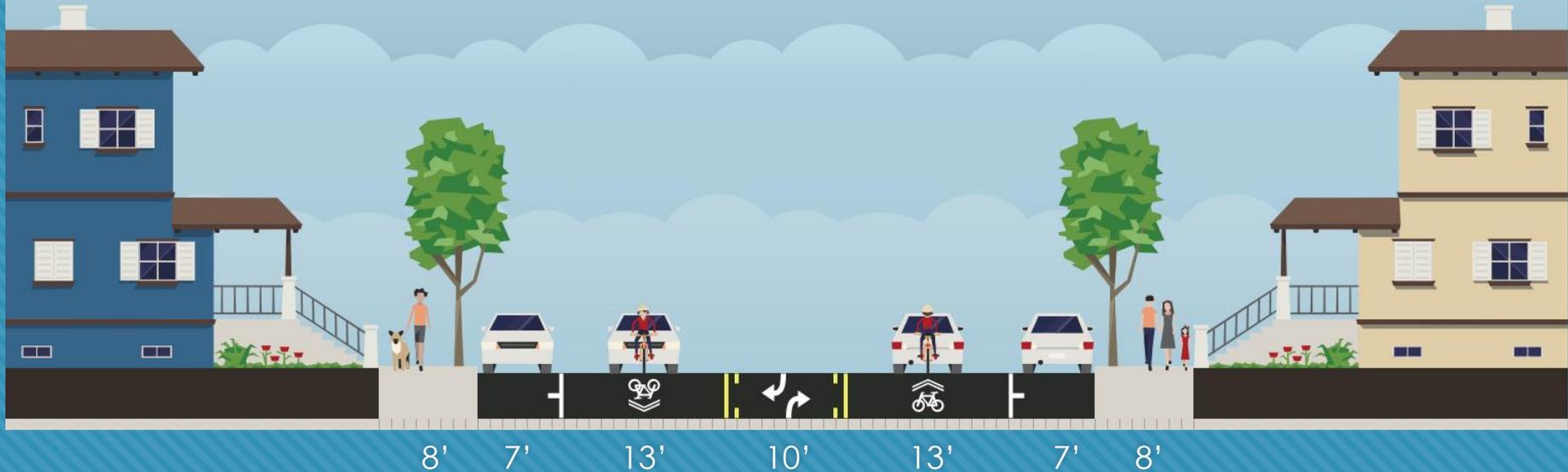
This typical section shows the project features for Alternative #2. The dimensions shown are preliminary and may change to accommodate local site conditions.

Alternative #2 wider sidewalk



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Right-of-way width: 66 ft.



- Bike lane
- Bike lane buffer
- Sidewalk (8 ft. wide)
- On-street parking
- Center Two-Way Left Turn Lane
- Raised median

This typical section shows the project features for Alternative #3. The dimensions shown are preliminary and may change to accommodate local site conditions.

Alternative #3 two-way left turn lane



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Right-of-way width: 66 ft.



- Bike lane
- Bike lane buffer
- Sidewalk (9.5 ft. wide)
- On-street parking
- Center Two-Way Left Turn Lane
- Raised median

This typical section shows the project features for Alternative #4. The dimensions shown are preliminary and may change to accommodate local site conditions.

Alternative #4 raised median island



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Summary of Alternatives



Project Feature	Alt. 1	Alt. 2	Alt. 3	Alt. 4
Designated Bike Lane	☑ Yes	☑ Yes	☒ No	☑ Yes
Bike Lane Buffer	☑ Yes	☒ No	☒ No	☑ Yes
Sidewalk	8'	9'	8'	9.5'
On-street Parking	☑ Yes	☑ Yes	☑ Yes	☒ No
Center Two-Way Left Turn Lane	☒ No	☒ No	☑ Yes	☒ No
Raised Median	☒ No	☒ No	☒ No	☑ Yes



Comparison of Alternatives



Benefits & Impacts	Alt. 1	Alt. 2	Alt. 3	Alt. 4
Right of way for the exclusive use of bicycles	✓ Class II	✓ Class II	✗ Sharrow	✓ Class II
No buffer will be less safe for bicyclists	✓ Has buffer	✗ No buffer	✗ No buffer	✓ Has buffer
Bicycle zone width (bike lane buffer)	5 ft. 2 ft.	5 ft. none	N/A	5 ft. 3 ft.
Dedicated lane for left turning vehicles	✗ No	✗ No	✓ Yes	✗ No
Median for traffic calming and aesthetics	✗ No	✗ No	✗ No	✓ Yes
Left turn access to driveways	✓ Yes	✓ Yes	✓ Yes	✗ No

Note:

- Two-way left turn lane can be placed in lieu of Class II bike lanes or on-street parking
- Two-way left turn lane moves left-turning traffic out of the through lane
- Median can be placed in lieu of on-street parking
- Median will make it less convenient to access businesses



Next Steps

- ❖ How to Reach Us
 - Call: (510) 670-5485
 - Email: info@acpwa.org
 - Website: www.acpwa.org
 - Enter “Lewelling” in the search bar
- ❖ Stay connected – Sign up for eSubscribe
- ❖ Business Community
 - Return questionnaire by March 15, 2019
- ❖ Attend the follow-up meeting in a few months

Discussion / Conclusion

- ❖ Community Discussion
 - Answer general questions
 - Collect feedback
- ❖ Continue conversations at exhibit tables

